

WHITE (Unidirectional)

Spacing	
18 metres	Main carriageway and 7.3 m link road lane markings (General use)
9 metres	(1) Main carriageway and 7.3 m link road lane markings where radius is less than 450 m. (2) Main carriageway and 7.3 m link road lane markings where fog is prevalent.

RED (unidirectional)

Spacing	
18 metres	Nearside edge of carriageways * (General use)
9 metres	(1) Nearside edge of carriageways * where radius is less than 450 m. (2) Nearside edge of carriageways * where fog is prevalent. (3) Nearside edge of merge and diverge – see Drawing No. D2.
3 metres	At both sides of nose at edge of carriageway * – see Drawing No. D2.

* (Main, Slip and Link Roads)

AMBER/RED (bidirectional) Amber reflectors to face oncoming traffic in normal conditions

Spacing	
18 metres	Offside edge of carriageways * (General use)
9 metres	(1) Adjacent to offside hatching when number of lanes reduced. (2) Offside edge of carriageways * where radius is less than 450 m. (3) Offside edge of carriageways * where fog is prevalent. (EXCEPTION: When adjoining chevron markings for nose at a merge or diverge – see Drawing No. D2.

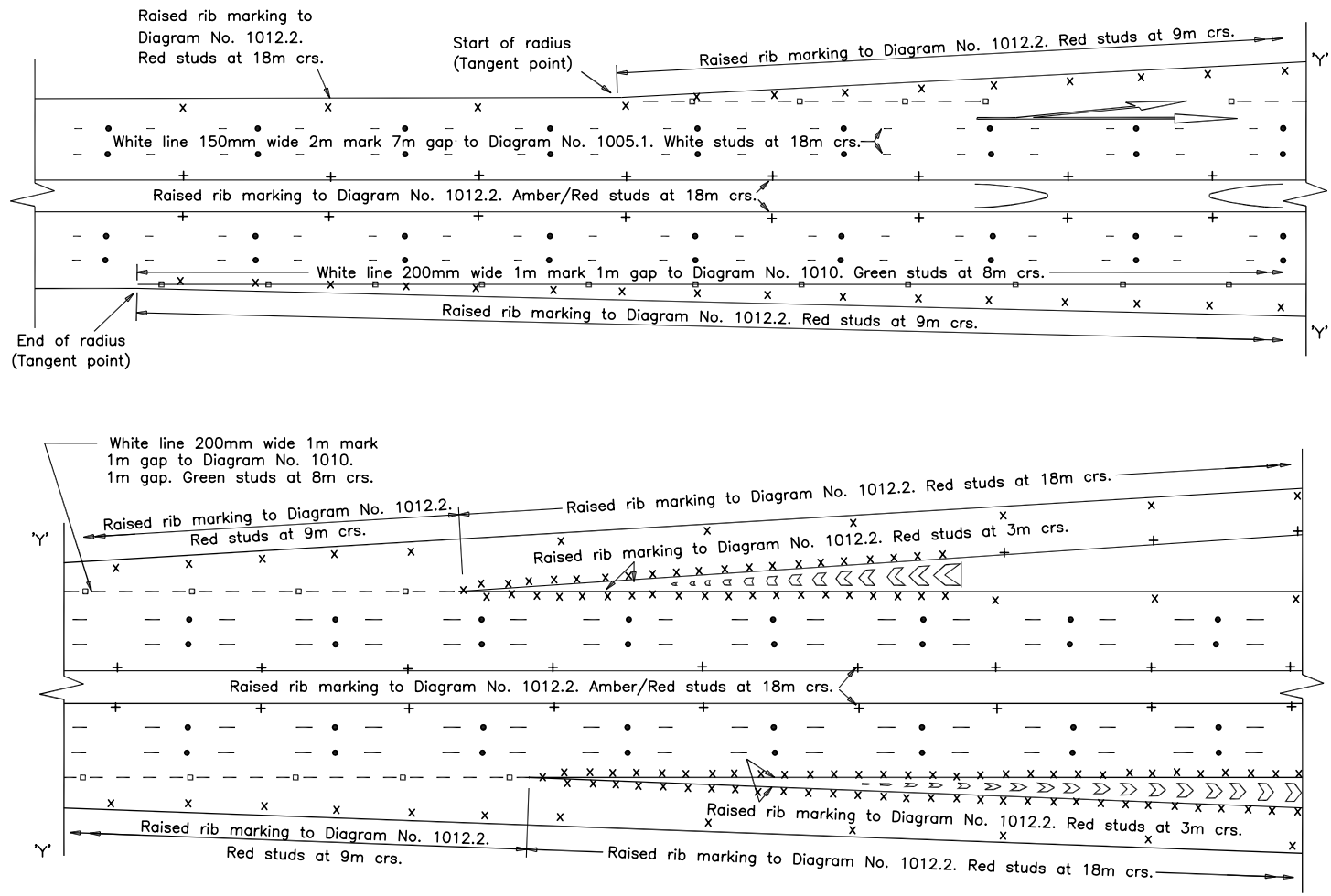
* (Main, Slip and Link Roads)

GREEN (unidirectional)

Spacing	
8 metres	Across merging/diverging tapers at standard junctions – see drawing No. D2
8 metres	At lane drop junctions, commencing at final ADS – see drawing No. D5
18 metres	At lane drop junctions, commencing at 1/2 mile ADS – see drawing No. D5

Studs should not be used in permanent positions in the constructed carriageway where temporary ends occur and where the studs will conflict with temporary arrangements

HIGHWAY CONSTRUCTION DETAILS	CARRIAGEWAY MARKINGS FOR RURAL MOTORWAYS	C	NOV 08	GENERAL NOTES FOR RETROREFLECTING ROAD STUDS	Drawing No.
		B	MAY 01		D1
		A	DEC 91		
		Issue	Date		



NOTES

1. Road markings shall comply with The Traffic Signs Regulations and General Directions 2002 (Statutory Instrument 2002 No. 3113) or The Traffic Signs Regulations (Northern Ireland) 1997.
2. These details do not apply where the number of lanes is being altered. In these cases the layout will be shown on the Drawings. See also Drawing No. D5.
3. For details of individual lane markings see the Drawings.
4. For details of chevron markings see Drawing No. D4.
5. For details and location of arrow see Drawing No. D3.
6. Where amber/red, red retroreflecting road studs are of the depressible they shall be positioned in relation to the edge of carriageway lines as shown on the plan. Where non-depressible studs are used they shall be positioned on the opposite side of the edge-lines so as to reduce the effects of over-running, unless otherwise specified in Appendix 12/3.

KEY (Retroreflecting Road Studs)
 x = Red Unidirectional
 • = White Unidirectional
 + = Amber/Red Bidirectional
 □ = Green Unidirectional

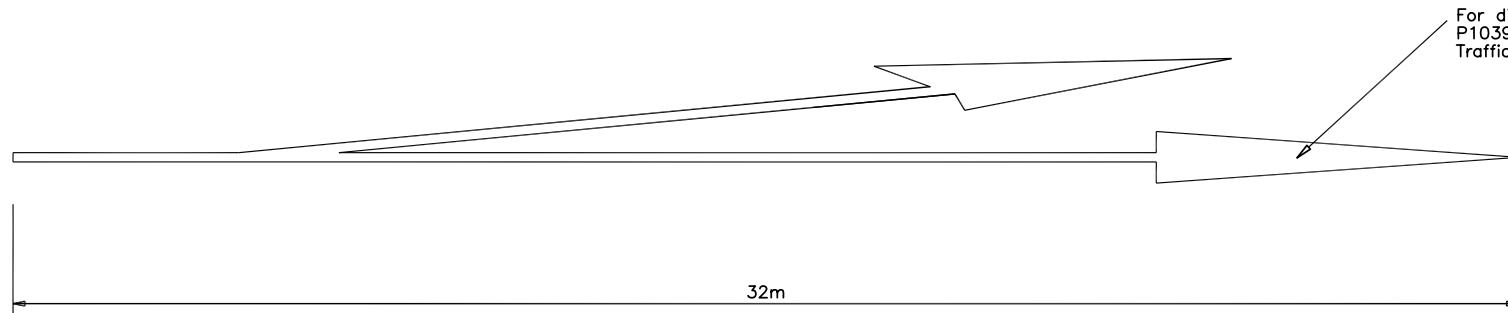
HIGHWAY CONSTRUCTION DETAILS

CARRIAGEWAY MARKINGS FOR RURAL MOTORWAYS

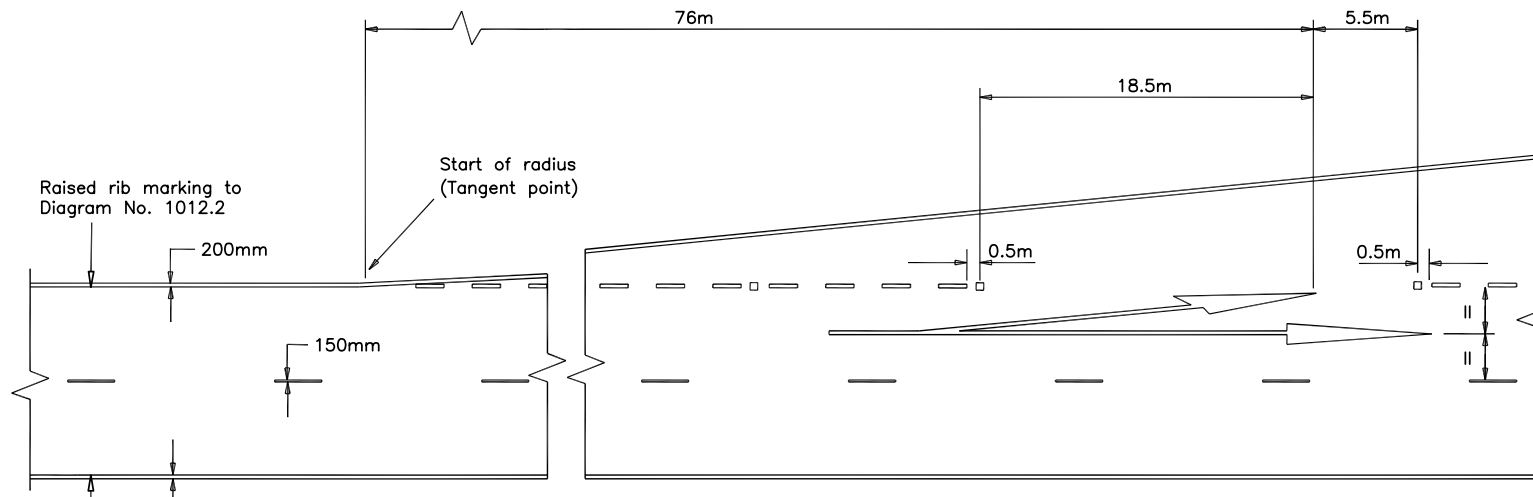
D	NOV 08
C	AUG 03
B	MAY 01
A	DEC 91
Issue	Date

TYPICAL LAYOUT OF MARKINGS FOR STANDARD MERGING AND DIVERGING LANES

Drawing No. D2



ARROW TO DIAGRAM No. 1039



LOCATION OF ARROW

KEY (Retroreflecting Road Stud)

□ Green Unidirectional

NOTE: Red, white and amber/red studs omitted for clarity.

NOTES

1. Road markings shall comply with The Traffic Signs Regulations and General Directions 2002 (Statutory Instrument 2002 No.3113) or The Traffic Signs Regulations (Northern Ireland) 1997.
2. This detail is associated with Drawing No. D2.

HIGHWAY CONSTRUCTION DETAILS

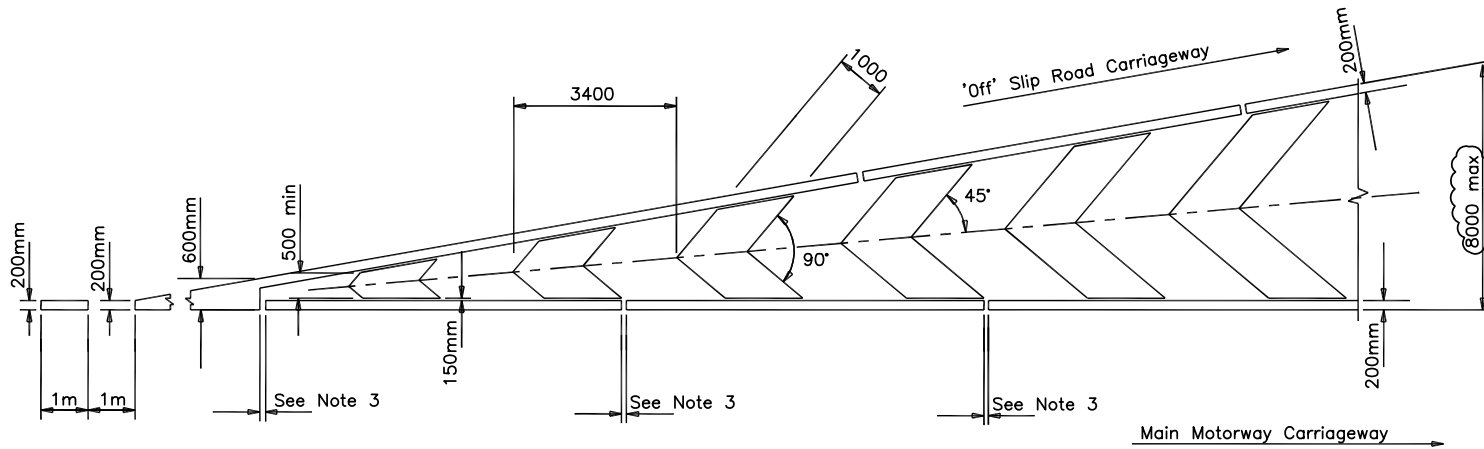
CARRIAGEWAY
MARKINGS FOR
RURAL MOTORWAYS

C	AUG 03
B	MAY 01
A	DEC 91
Issue	Date

DETAIL AND TYPICAL LOCATION
OF ARROW TO INDICATE TO TRAFFIC
THE ENTRANCE TO A STANDARD
DIVERGING LANE

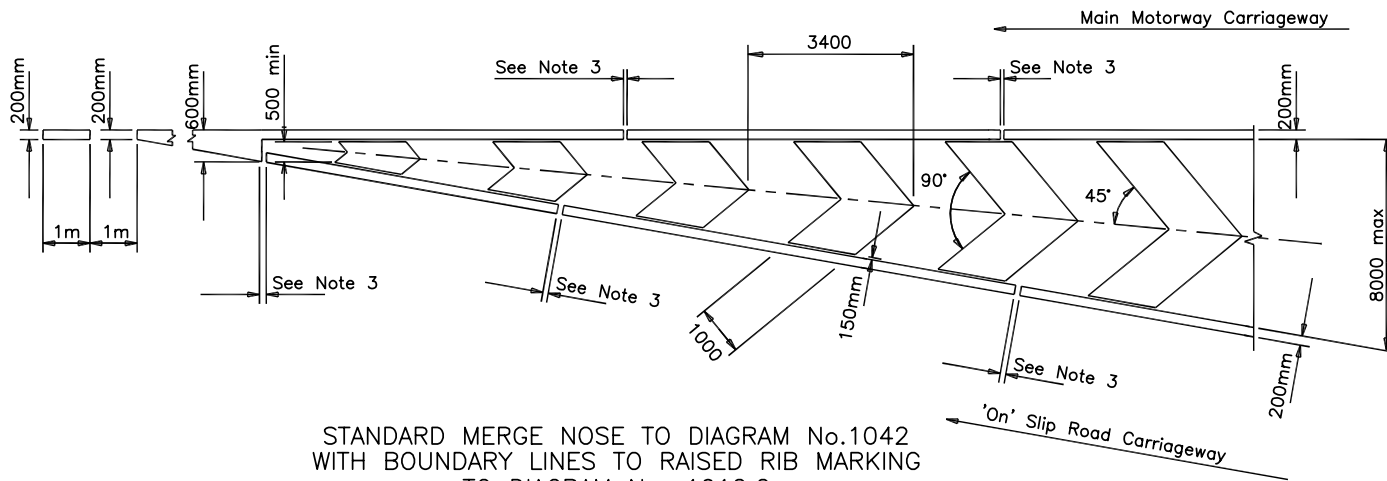
Drawing No.

D3



STANDARD DIVERGE NOSE TO DIAGRAM No.1042
WITH BOUNDARY LINES TO RAISED RIB MARKING
TO DIAGRAM No. 1012.2

NOTE:
On both details studs
omitted for clarity.



STANDARD MERGE NOSE TO DIAGRAM No.1042
WITH BOUNDARY LINES TO RAISED RIB MARKING
TO DIAGRAM No. 1012.2

NOTES

1. Road markings shall comply with The Traffic Signs Regulations and General Directions 2002 (Statutory Instrument 2002 No. 3113) or The Traffic Signs Regulations (Northern Ireland) 1997.
2. This detail is associated with Drawing Nos. D2 and D5.
3. The boundary lines may be provided with 25 to 50mm gaps at irregular intervals, where ponding is expected, to promote free surface water drainage.

HIGHWAY CONSTRUCTION DETAILS

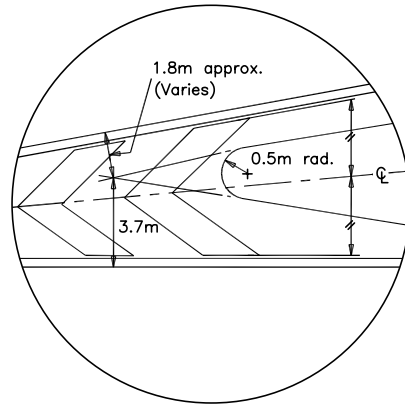
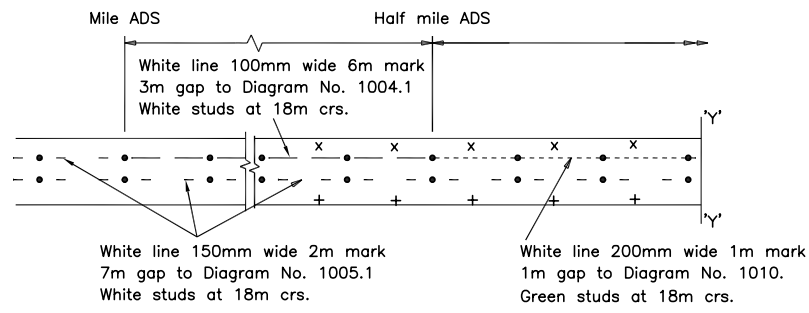
CARRIAGEWAY
MARKINGS FOR
RURAL MOTORWAYS

C	AUG 03
B	MAY 01
A	DEC 91
Issue	Date

TYPICAL CHEVRON MARKINGS FOR
STANDARD NOSES

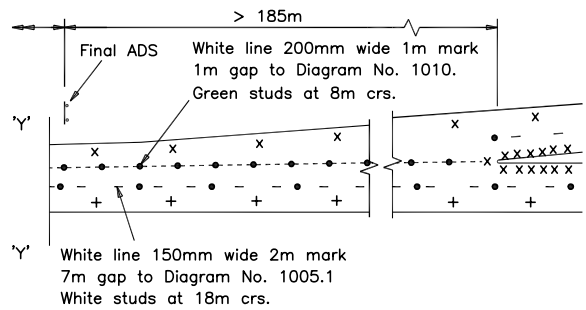
Drawing No.

D4

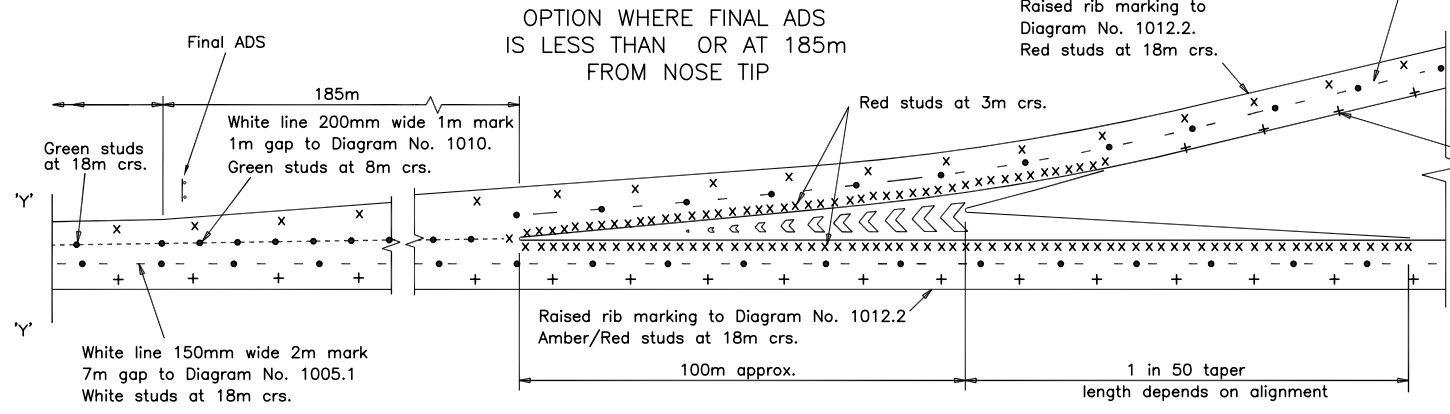


DETAIL OF BACK OF NOSE

- NOTES
- Road markings shall comply with The Traffic Signs Regulations and General Directions 2002 (Statutory Instrument 2002 No.3113) or The Traffic Signs Regulations (Northern Ireland) 1997.
 - Details of chevron markings as for standard diverge nose – see Drawing No. D4.



OPTION WHERE FINAL ADS IS GREATER THAN 185m FROM NOSE TIP



OPTION WHERE FINAL ADS IS LESS THAN OR AT 185m FROM NOSE TIP

- White line 100mm wide 2m mark
7m gap to Diagram No. 1005.1.
White studs at 18m crs.
- Raised rib marking to Diagram No. 1012.2.
Red studs at 18m crs.
- Raised rib marking to Diagram No. 1012.2.
Amber/Red studs at 18m crs.
- KEY (Retroreflecting Road Studs)
x = Red Unidirectional
• = White Unidirectional
+ = Amber/Red Bidirectional

HIGHWAY CONSTRUCTION DETAILS	CARRIAGEWAY MARKINGS FOR RURAL MOTORWAYS	E	NOV 08	TYPICAL CARRIAGEWAY DIVERGENCE WITH LANE REDUCTION ON MAIN CARRIAGEWAY	Drawing No.
		D	AUG 03		D5
C	MAY 01				
B	MAR 99				
A	DEC 91				
Issue	Date				

White line 150mm wide 2m mark
7m gap to Diagram No. 1005.1.
White studs at 18m crs

Raised rib marking to
Diagram No. 1012.2
Amber/Red studs at 9m crs

3 No warning arrows (spaced at
2 and 3 seconds of travel time
for 120kph [70mph] design speed)

Raised rib marking to
Diagram No. 1012.2
Red studs at 18m crs

Hatched marking to
Diagram No. 1040.3

See Note 3
200m

300m

Amber/Red studs at 9m crs

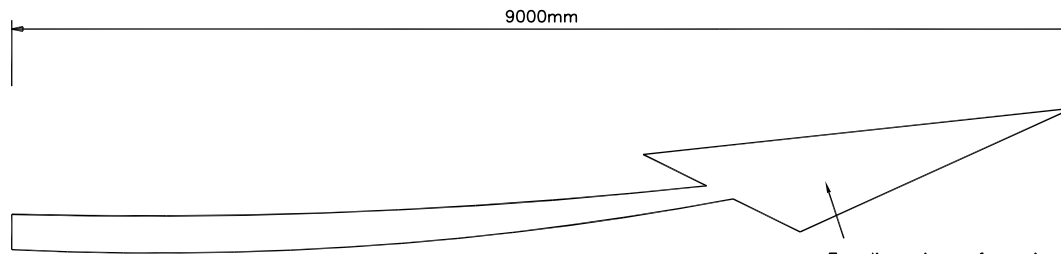
KEY (Retroreflecting Road Studs)
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CARRIAGEWAY LANE REDUCTION

3 to 2 lanes. Termination of offside lane shown.
Authorization shall be required for reverse
hatching for termination of nearside lane
(in this case the nearside studs will be red at 9m
centres adjacent to the hatching)

NOTES

- Road markings shall comply with The Traffic Signs Regulations and General Directions 2002 (Statutory Instrument 2002 No. 3113) or The Traffic Signs Regulations (Northern Ireland) 1997.
- Wherever possible lane reduction should take place at interchanges (see Drawing No. D5).
- The boundary lines may be provided with 25 to 50mm gaps at irregular intervals, where ponding is expected, to promote free surface water drainage.



For dimensions of warning arrow
see Drawing P1014 in Working
Drawings for Traffic Sign Design
and Manufacture

WARNING ARROW TO DIAGRAM No. 1014

HIGHWAY CONSTRUCTION DETAILS

CARRIAGEWAY
MARKINGS FOR
RURAL MOTORWAYS

D	NOV 08
C	AUG 03
B	MAY 01
A	DEC 91
Issue	Date

TYPICAL LANE REDUCTION
(3 TO 2 LANE)
AND DETAIL OF WARNING ARROW

Drawing No.

D6